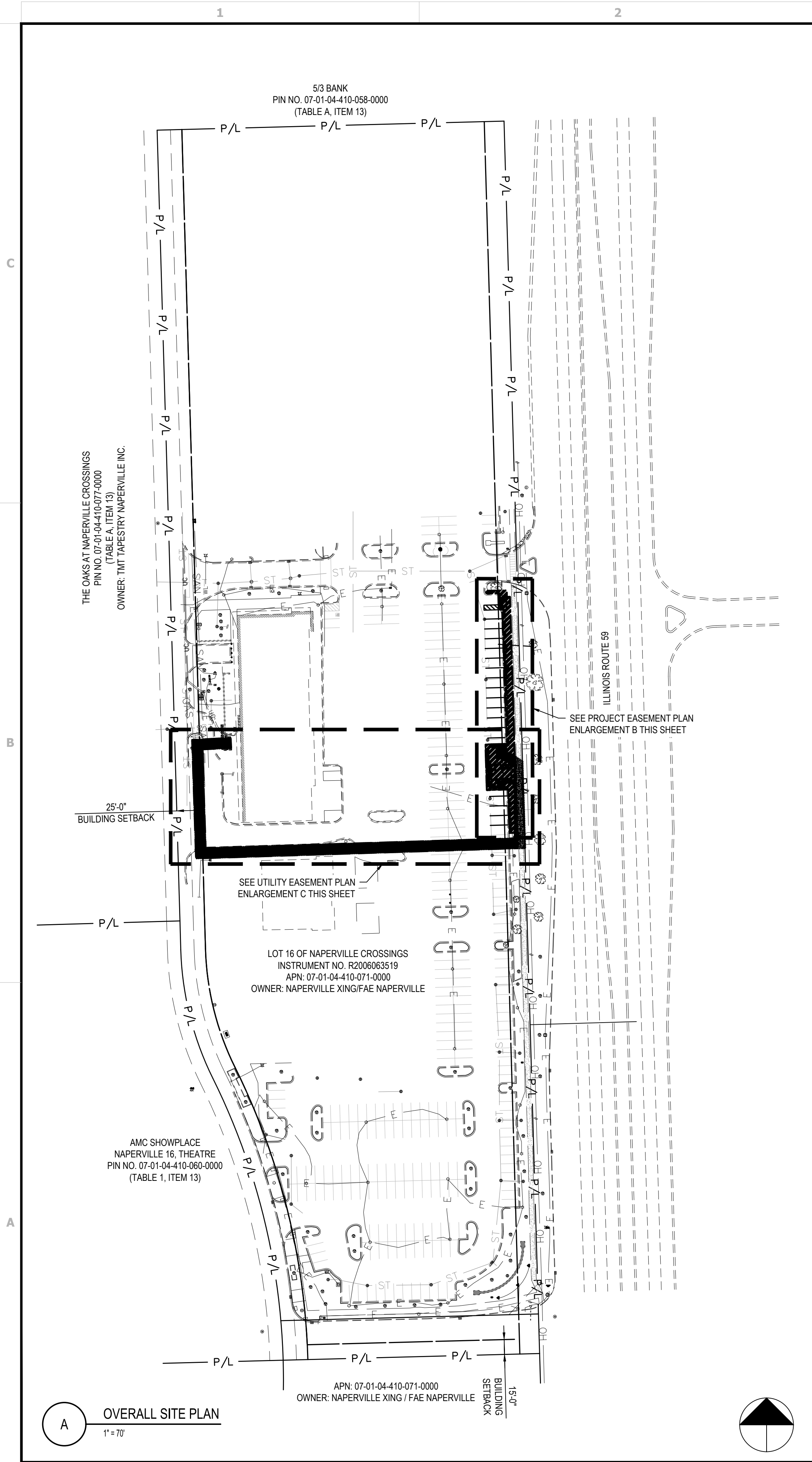
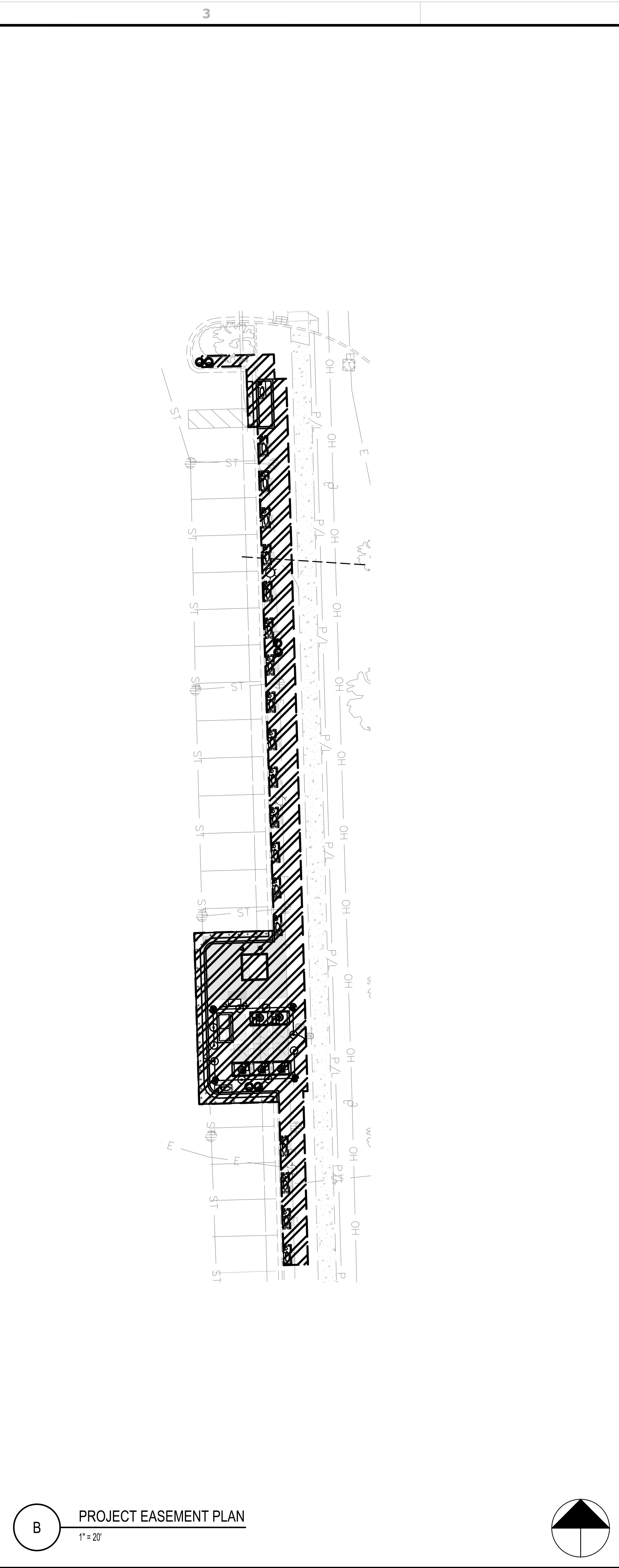


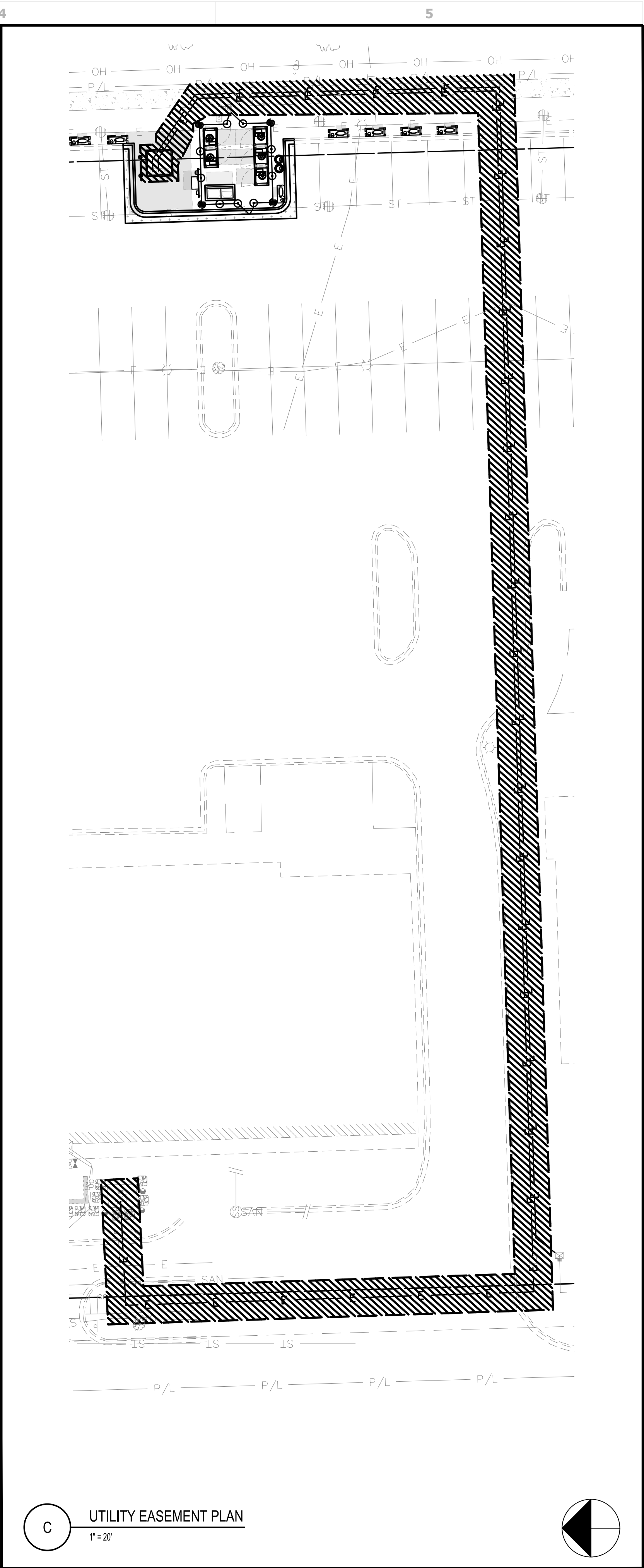
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 March 6, 2024, 3:58 PM - shoneyatt



A OVERALL SITE PLAN
 1" = 70'



B PROJECT EASEMENT PLAN
 1" = 20'



C UTILITY EASEMENT PLAN
 1" = 20'

TESLA
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2	03/07/2024	ADDED LANDSCAPE PLAN

LEONARDO A. SFERRA
 LICENSED PROFESSIONAL ENGINEER
 STATE OF ILLINOIS
 062.069126

Leonardo A. Sferra
 Signature
 03/11/24
 Date
 11/30/2025
 Exp. Date

TESLA SUPERCHARGER STATION
 2724 SHOWPLACE DRIVE
 NAPERVILLE, IL 60564

**OVERALL
 CIVIL SITE PLAN**

PROJECT MANAGER	DESIGNER
IM	NSE

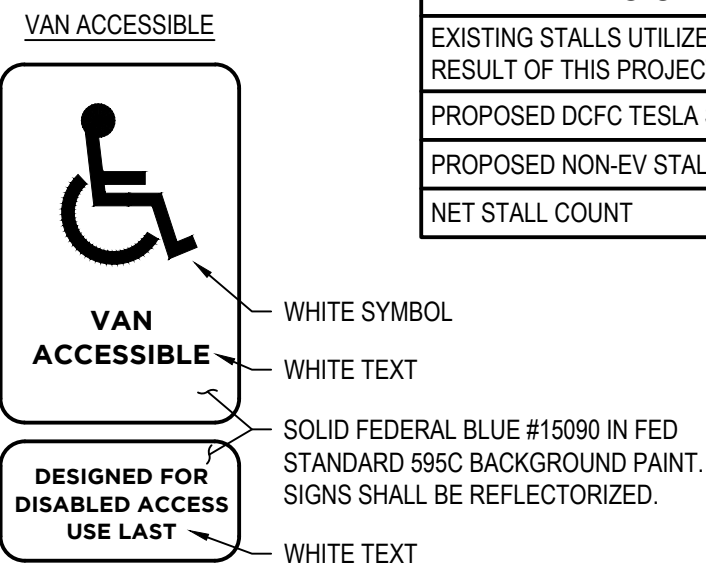
JOB NO.
2023141.99

C-110
 PLANNING# 23-23-10000100
 BUILDING# 24-34

SUPERCHARGER POST SCHEDULE

CABINET	POST	SIGN TYPE
1	1A	VAN ACCESSIBLE
	1B	N/A
	1C	N/A
	1D	N/A
2	2A	N/A
	2B	N/A
	2C	N/A
	2D	N/A
3	3A	N/A
	3B	N/A
	3C	N/A
	3D	N/A
4	4A	N/A
	4B	N/A
	4C	N/A
	4D	N/A
5	5A	N/A
	5B	N/A
	5C	N/A
	5D	N/A

SIGNAGE



PARKING STALL ANALYSIS

EXISTING STALLS UTILIZED AS A RESULT OF THIS PROJECT	25
PROPOSED DCFC TESLA STALLS	20
PROPOSED NON-EV STALLS	0
NET STALL COUNT	-5

GENERAL SHEET NOTES

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- CONTRACTOR SHALL REMOVE EXISTING PAVEMENT AND/OR CURB USING CLEAN SAWCUTS TO INSTALL PROPOSED UNDERGROUND CONDUITS AND REPLACE PAVEMENT AND/OR CURB AFTER CONDUITS HAVE BEEN INSTALLED. SEE ELECTRICAL SHEETS FOR CONDUIT ROUTING, APPROXIMATE CONDUIT RUN LENGTHS AND TRENCH DETAIL. CONTRACTOR SHALL MEET OR EXCEED EXISTING PAVEMENT SPECIFICATIONS. NOTIFY ENGINEER OF ANY DISCREPANCIES PRIOR TO PERFORMING WORK.
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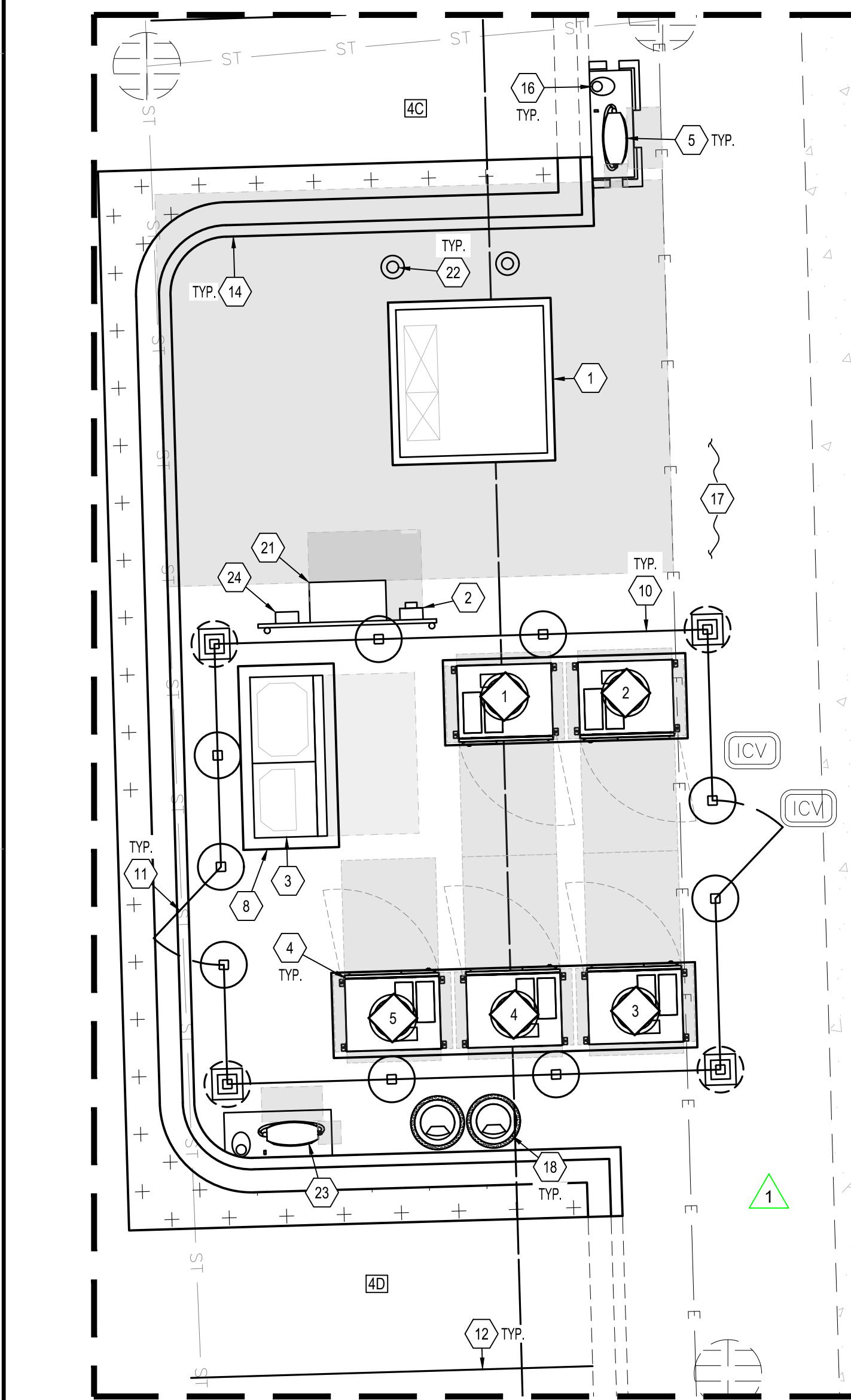
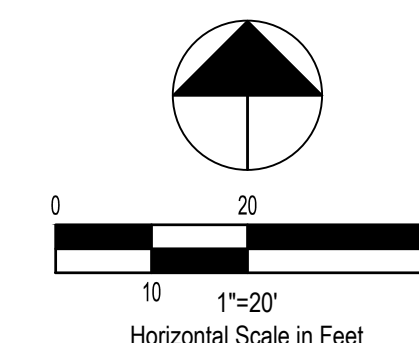
PLAN KEYNOTES

- PROPOSED PAD MOUNTED ELECTRICAL UTILITY TRANSFORMER (BY UTILITY). CONTRACTOR SHALL PROVIDE CONCRETE PAD PER UTILITY SPECIFICATIONS. COORDINATE FINAL LOCATION WITH UTILITY. SEE ELECTRICAL PLANS FOR PROPOSED ROUTING.
- PROPOSED UTILITY METER MOUNTED ON H-FRAME PER ELECTRIC COMPANY SPECIFICATIONS AND DETAILS ON ELECTRICAL SHEETS.
- PROPOSED SWITCHGEAR ASSEMBLY WITH INTEGRATED TESLA SITE CONTROLLER AND PRIMARY BROADCAST UNIT PER ELECTRICAL DRAWINGS. SEE SHEET C-201, DETAIL B-4.
- PROPOSED TESLA SUPERCHARGER CABINET (TYPICAL OF 5). SEE CIVIL DETAILS.
- PROPOSED TESLA SUPERCHARGER POST WITH INDIVIDUAL PRECAST CONCRETE FOUNDATION (TYPICAL OF 18). SEE SHEET C-201, DETAIL C-2.
- PROPOSED TESLA SUPERCHARGER POST WITH CAST-IN-PLACE CONCRETE FOUNDATION (TYPICAL OF 1). INSTALL POST FLUSH WITH PAVEMENT. SEE SHEET C-202, DETAIL A-1.
- PROPOSED NON-ILLUMINATED PARKING SIGN (TYPICAL OF 1). SEE SHEET C-201, DETAIL B-2. SEE SUPERCHARGER POST SCHEDULE, THIS SHEET. MOUNT SIGN POST IN BOLLARD AS NOTED.
- PROPOSED CONCRETE EQUIPMENT PAD. SEE SHEET C-201, DETAIL C-4.
- PROPOSED RELOCATED LIGHT POLE (TOTAL OF 1). SEE SHEET C-202, DETAIL B-1. SEE ELECTRICAL DRAWINGS FOR POLE AND FIXTURE SPECIFICATIONS AND WIRING.
- PROPOSED TREX 'SECLUSION' FENCING WITH BRICK/STEEL PILLARS. SEE SHEET C-202, DETAIL C-4.
- PROPOSED TREX 'SECLUSION' SINGLE GATE WITH KEYPAD AND PANIC HARDWARE (TYPICAL OF 2). SEE SHEET C-202, DETAIL B-3 & C-2.
- PROPOSED PAINTED 4" WIDE SOLID STRIPE TO MATCH EXISTING STRIPING IN COLOR. SEE PAVEMENT MARKING NOTES ON SHEET C-003.
- PROPOSED PAINTED 4" WIDE TRANSVERSE STRIPING TO MATCH EXISTING STRIPING IN COLOR. STRIPING SHALL BE 3'-0" O.C. SEE PAVEMENT MARKING NOTES ON SHEET C-003 AND SHEET C-201, DETAIL A-5.
- PROPOSED CONCRETE CURB AND REVERSE PITCH GUTTER. SEE SHEET C-201, DETAIL A-3.
- PROPOSED FLUSH CONCRETE CURB AND REVERSE PITCH GUTTER WITH INTEGRATED CONCRETE WALK. SEE SHEET C-201, DETAIL A-4.
- PROPOSED REBOUNDING DETERRENT BOLLARD (TYPICAL OF 20). SEE SHEET C-201, DETAIL A-2.
- PROPOSED LANDSCAPE SEE SHEET L-101.
- PROPOSED TRASH CAN AND RECYCLING BIN (TYPICAL OF 3 EACH) MOUNTED ON CONCRETE PAD. SEE SHEET C-201, DETAIL C-1.
- PROPOSED SECONDARY BROADCAST UNIT (TYPICAL OF 1). MOUNT PER MANUFACTURER'S SPECIFICATIONS AT MINIMUM 10'-0" ABOVE GRADE. WHERE APPLICABLE, CONTRACTOR SHALL MOUNT TO EXISTING LIGHT POLE.
- PROPOSED 6" CONCRETE CURB. SEE SHEET C-201, DETAIL C-5.
- PROPOSED CT CABINET ON H-FRAME PER UTILITY SPECIFICATIONS.
- PROPOSED DETERRENT BOLLARD PER UTILITY SPECIFICATIONS (TYPICAL OF 2).
- PROPOSED TESLA SUPERCHARGER POST WITH CAST-IN-PLACE CONCRETE FOUNDATION (TYPICAL OF 1). SEE SHEET C-201, DETAIL B-2.
- PROPOSED REMOTE POWER SWITCH MOUNTED ON H-FRAME. SEE ELECTRICAL DETAILS.

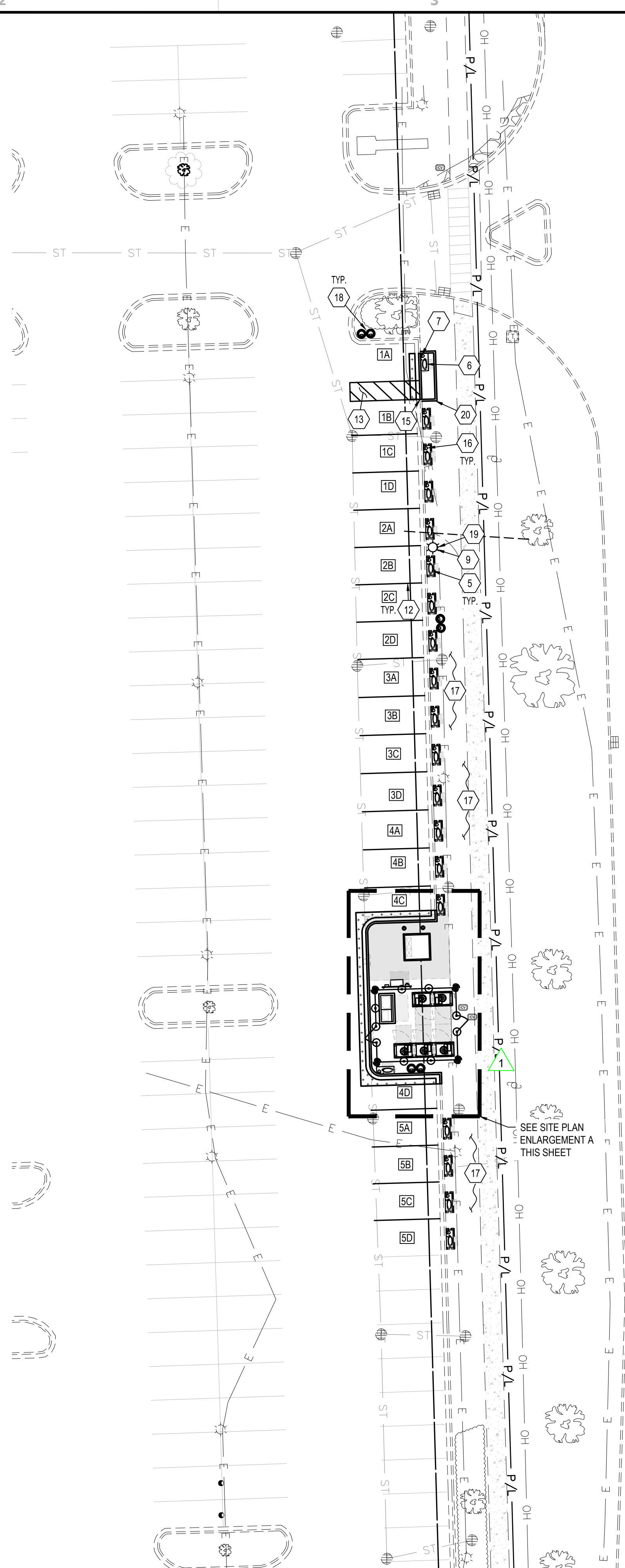
LEGEND

(SEE SHEET C-003 FOR EXISTING LEGEND)

- PROPOSED EQUIPMENT CLEAR SPACE
- PROPOSED ASPHALT PAVEMENT TO MATCH EXISTING IN TYPE AND DEPTH. INCLUDE ENGINEERED COMPACTED BACKFILL BELOW PAVEMENT SECTION. TRENCHING NOT INCLUDED. FOR FULL DEPTH REPLACEMENT, CONTRACTOR SHALL REPLACE ANY FABRIC ENCOUNTERED DURING EXCAVATION INCLUDING BUT NOT LIMITED TO; GEOTEXTILE, WATER-PROOFING, PAVING FABRICS, ETC. THE REPLACEMENT MATERIAL(S) SHALL BE EQUAL TO OR BETTER THAN EXISTING AND SHALL BE CONFIRMED BY THE MANUFACTURER'S REPRESENTATIVE TO BE COMPATIBLE WITH THE EXISTING INSTALLATION. IN LIEU OF FULL DEPTH REPLACEMENT, CONTRACTOR CAN MILL AND OVERLAY (1.5" MIN.) PROPOSED ASPHALT PROVIDED THAT THE FINAL ASPHALT SECTION IS EQUAL TO OR GREATER THAN THE EXISTING SECTION AND DRAINAGE AND ADA COMPLIANCE IS NOT NEGATIVELY AFFECTED.



A SITE PLAN ENLARGEMENT
1" = 5'



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Leonardo A. Sferra
Signature
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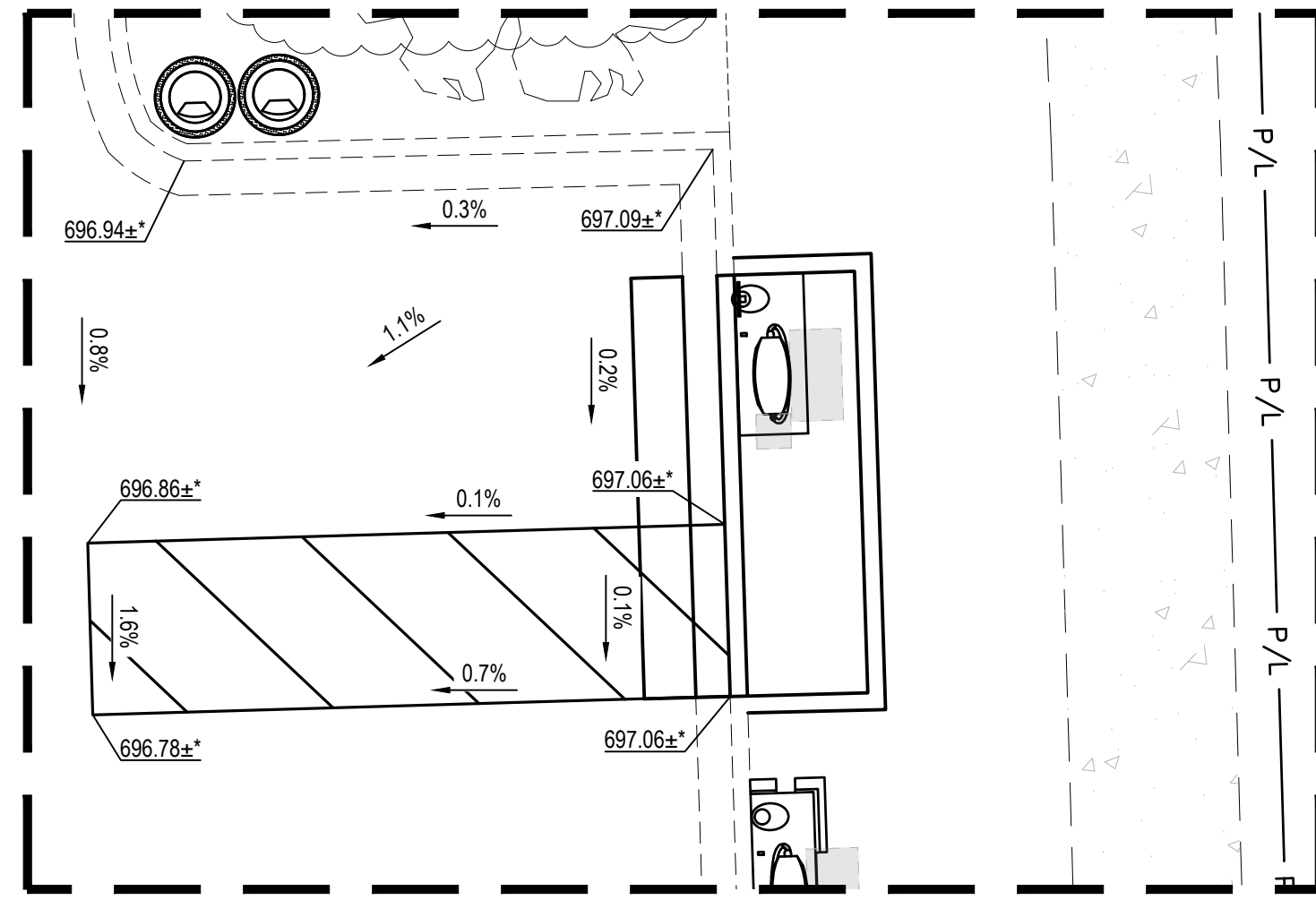
PROJECT MANAGER	DESIGNER
IM	NSE

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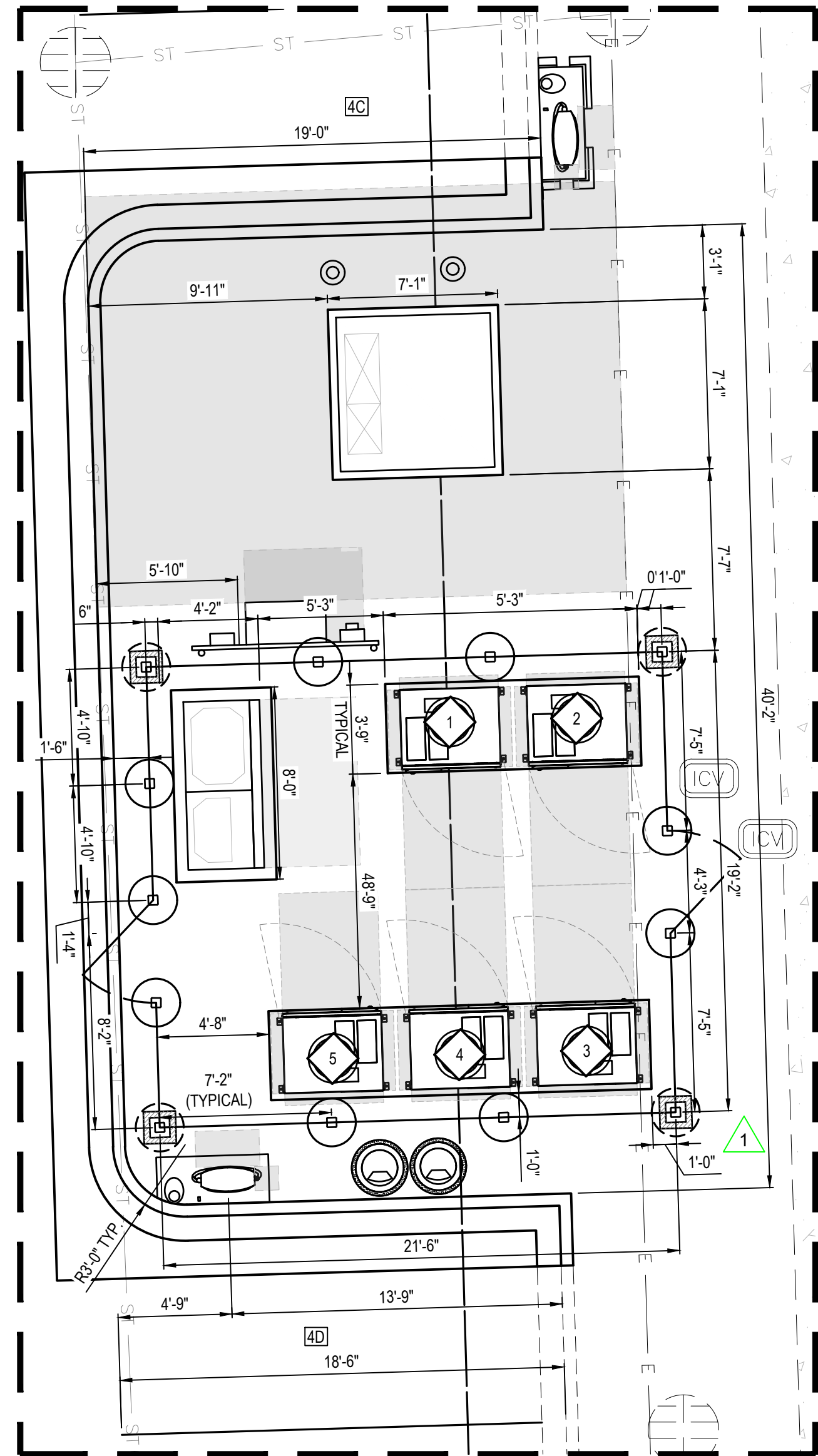
C-111
PLANNING# 23-23-10000100
BUILDING# 24-340

CIVIL SITE PLAN

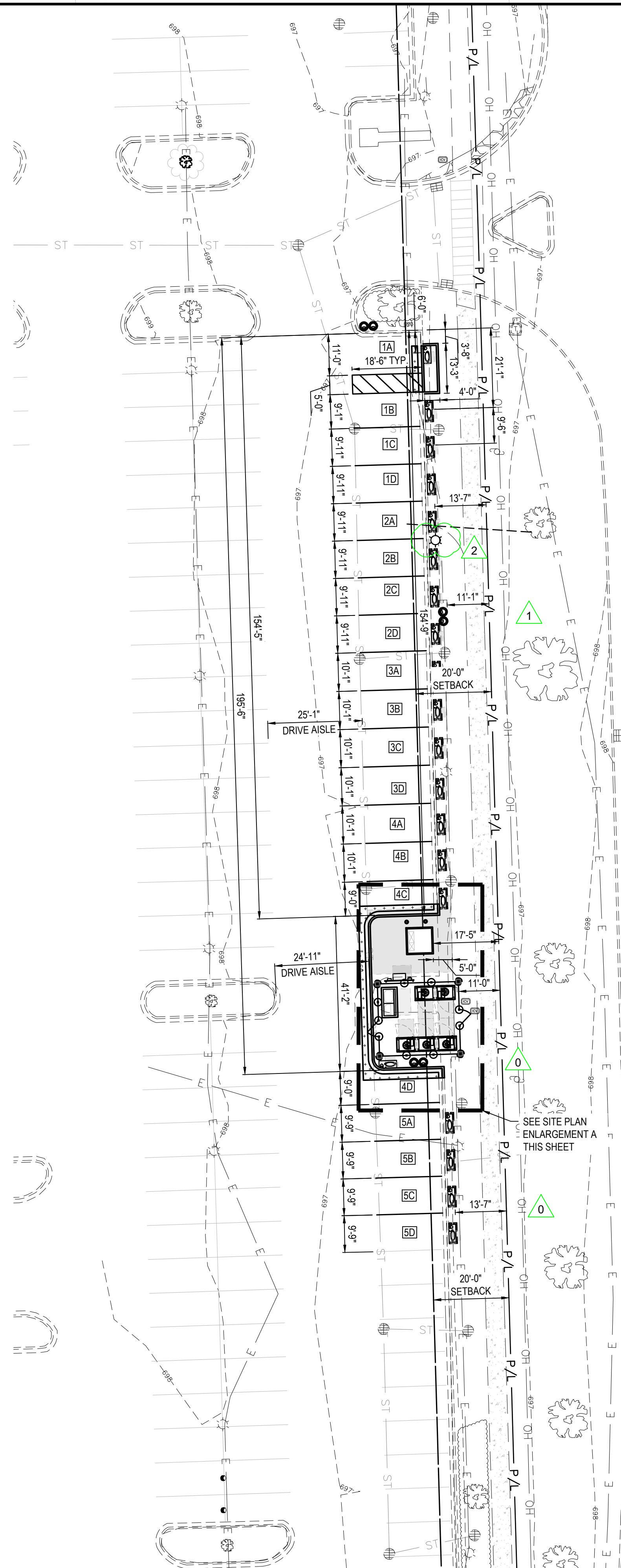
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 March 8, 2024, 3:58 PM - shoneya



B ADA STALL ENLARGEMENT
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GRADING LEGEND

- 000.00± MATCH EXISTING ELEVATION
- 0.0% EXISTING DRAINAGE SLOPE & ARROW



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DIMENSION SITE PLAN

PROJECT MANAGER	DESIGNER
IM	NSE

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C-112
 PLANNING# 23-23-10000100
 BUILDING# 24-340